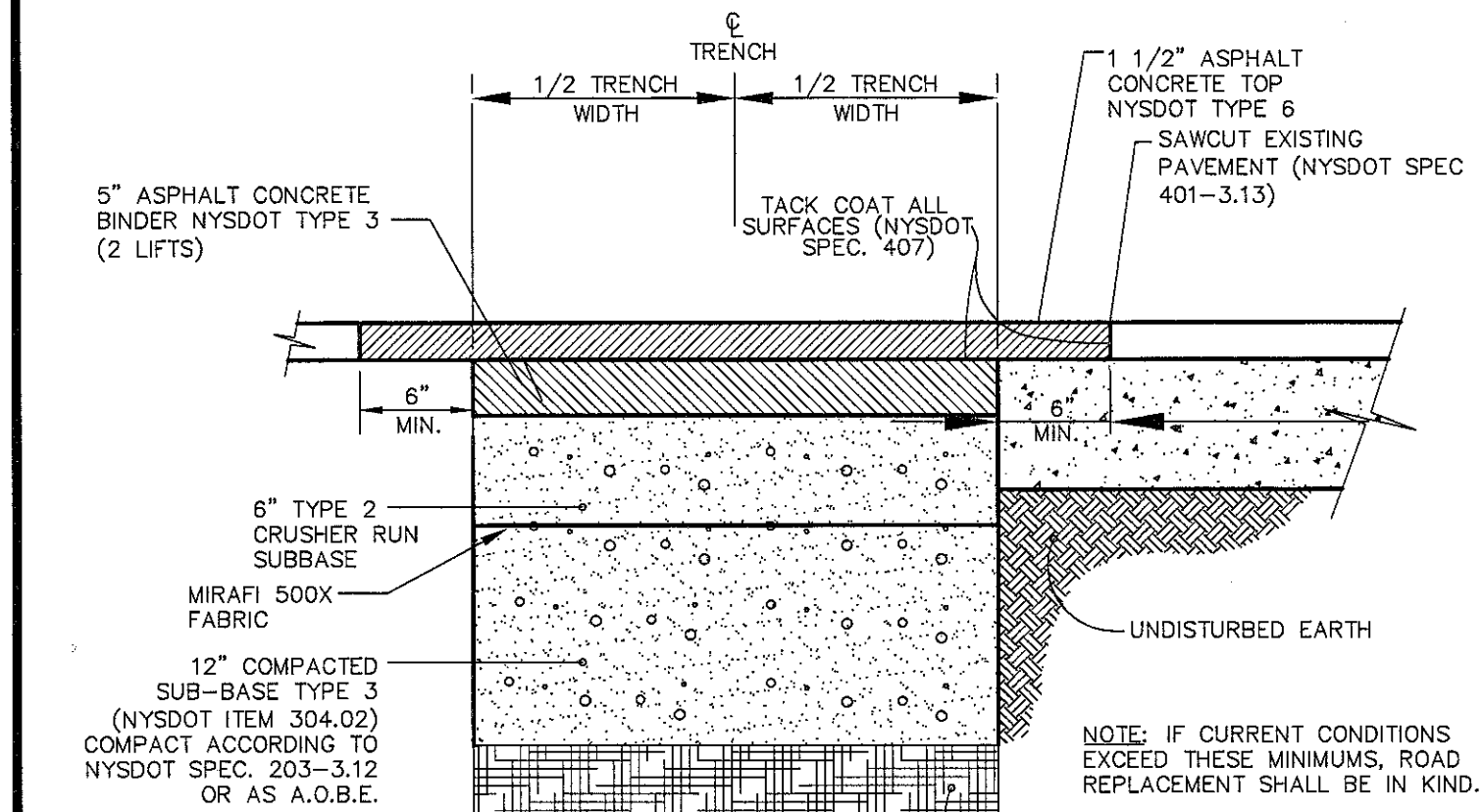


PLAN VIEW
SCALE 1" = 20'



PAVEMENT RESTORATION
(WITHIN TOWN R.O.W.)
N.T.S.

GENERAL EMBANKMENT NOTES

- EMBANKMENT MATERIAL SHALL BE REVIEWED AND APPROVED BY THE ENGINEER PRIOR TO PLACEMENT.
- COMPACTION SHALL BE 95% OF MAXIMUM DENSITY.
- THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER FOR REVIEW, THE METHOD OF COMPACTION AND EQUIPMENT TO BE USED.
- EMBANKMENT MATERIALS SHALL CONSIST OF SOILS CLASSIFIED AS CLAY (CL) WITH A MINIMUM PERMEABILITY OF 1×10^{-5} CM/SEC. EMBANKMENT MATERIALS SHALL BE SUBMITTED, REVIEWED, AND APPROVED BY ENGINEER PRIOR TO CONSTRUCTION.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO PROPERLY PLACE AND COMPACT ALL MATERIALS IN THE EMBANKMENTS, BERM, BOTH INTERIOR AND EXTERIOR, AND ANY ACCESS ROADS AS SHOWN ON THE CONTRACT DRAWINGS. PLACEMENT OF MATERIALS SHALL BE TO THE ELEVATIONS, DIMENSIONS AND GRADES AS INDICATED ON THE CONTRACT DRAWINGS. ALL PLACED MATERIALS SHALL BE COMPACTED TO 95% STANDARD PROCTOR MAXIMUM DENSITY. COMPACTION TESTING WILL BE REQUIRED FOR EVERY 500 CUBIC YARDS OF MATERIAL PLACED, STARTING WITH THE FIRST LIFT. CONTRACTOR SHALL EMPLOY AN INDEPENDENT SOIL TESTING FIRM TO PERFORM IN-PLACE SOIL DENSITY TESTS AND FURNISH WRITTEN TEST RESULTS TO THE ENGINEER.

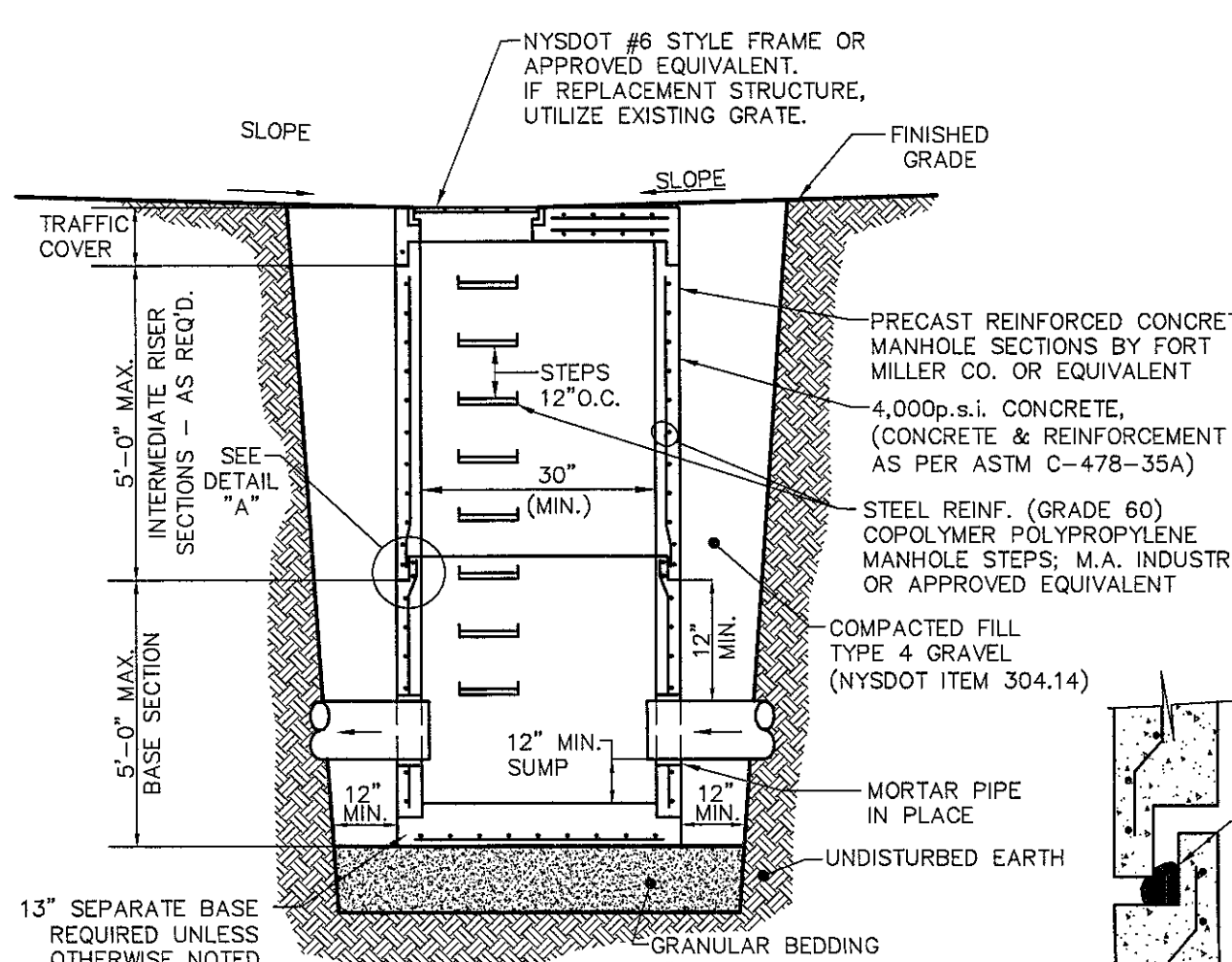
GRAVEL STORAGE AREA
AND ROAD SECTION
N.T.S.

SIGN SCHEDULE

SIGN ID, SYMBOL	SIGN TEXT	SIGN SIZE	TEXT SIZE & COLOR	NO. REQ'D.	TYPE OF MOUNTING	MUTCD NO.
1	STOP	30" x 30"	PER MUTCD	1	GROUND MOUNTED METAL POSTS	RI-1
2	NO LEFT TURN	18" x 24"	PER NYSDOT REGULATIONS	2	GROUND MOUNTED METAL POSTS	

SIGN NOTES:

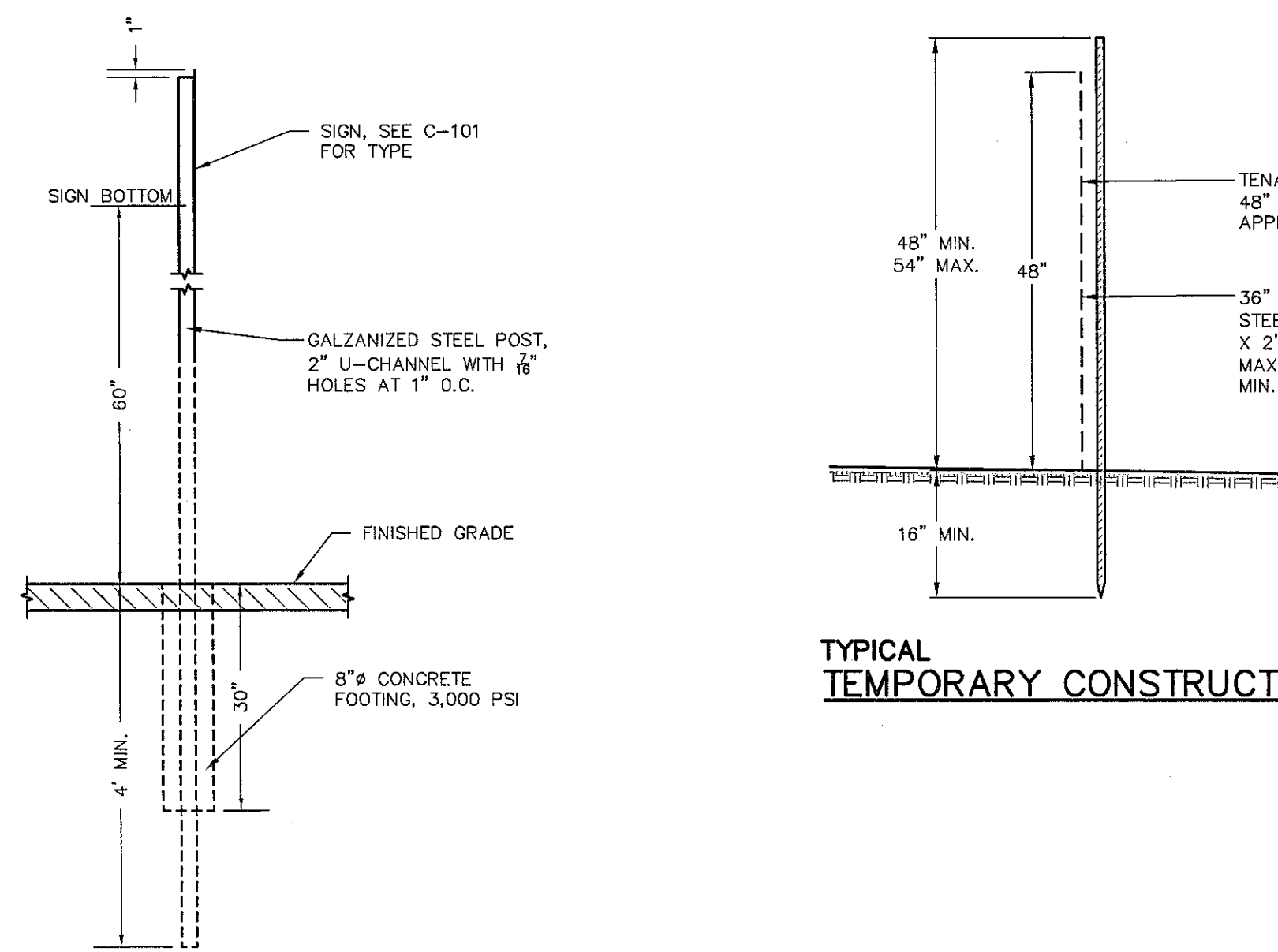
- SIGNS & PAVEMENT MARKINGS TO BE INSTALLED IN ACCORDANCE WITH THE LATEST EDITION OF THE NEW YORK STATE DEPARTMENT OF TRANSPORTATION MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES OR NEW YORK DEPARTMENT OF ENVIRONMENTAL CONSERVATION STORMWATER MANAGEMENT DESIGN MANUAL.
- UNLESS OTHERWISE SHOWN, ALL TRAFFIC SIGNS, INFORMATIONAL SIGNS AND SUPPORTS SHALL BE IN ACCORDANCE WITH THE NEW YORK STATE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS SECTION 645 "GUIDE SIGNS, TRAFFIC SIGNS AND SPECIAL DEVICES". SIGN PANELS SHALL BE ALUMINUM SUPPORT POLES SHALL BE GALVANIZED STEEL AND IN ACCORDANCE WITH NYSDOT STANDARD SHEET 645-7 & 645-8, LATEST VERSION. ALL SUPPORT POLES SHALL BE DRIVEN TO A DEPTH OF AT LEAST 3'-2" IN SOIL OR EMBEDDED 1'-0" INTO SOUND ROCK IF ENCOUNTERED ABOVE THIS DEPTH AND SHALL BE DESIGNED FOR LOCAL WIND LOADS. CONTRACTOR SHALL SUBMIT SHOP DRAWINGS OF ALL PROPOSED SIGNS AND THEIR INTENDED LOCATIONS TO THE ENGINEER.
- SIGNS SHALL BE MOUNTED AT APPROXIMATELY RIGHT ANGLES TO THE DIRECTION OF TRAFFIC AND TURNED SLIGHTLY AWAY FROM THE ROADWAY TO AVOID MIRROR REFLECTION OF VEHICLE HEADLIGHTS.
- STOP LINE SHALL BE A MINIMUM OF 18 INCHES WIDE.



PRECAST CONCRETE CATCH BASIN
N.T.S.

DETAIL A

SIGN POST INSTALLATION N.T.S.



TYPICAL TEMPORARY CONSTRUCTION FENCING
N.T.S.

OUTLET CONTROL STRUCTURE (OCS-3)
N.T.S.

REFER TO STORMWATER POLLUTION PREVENTION PLAN AND STORMWATER MANAGEMENT REPORT WATER QUALITY CALCULATIONS.

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